

Committee:	Date:
Planning Applications Sub Committee	11th February 2025
Subject: River Foreshore Adjacent to Riverbank House Swan Lane EC4R 3BF Proposed removal/cutting down to bed level of the existing campshed timbers. Removal of gabion baskets and steel tie rods between the existing campshed and anchor posts. Installation of new sloped revetment formed from imported granular fill and rock mattresses. Installation of Rock Bags in the gap that has formed between two of the sheet pile campshed retaining walls at the western end of the site. Reinstatement of the bed level behind the installed rock bags to match the top level of the sheet piles.	Public
Ward: Dowgate	For Decision
Registered No: 24/00938/FULLR3	Registered on: 11 September 2024
Conservation Area: No	Listed Building: No

Summary

Planning permission is sought for the proposed removal/cutting down to bed level of the existing campshed timbers; removal of gabion baskets and steel tie rods between the existing campshed and anchor posts; installation of new sloped revetment formed from imported granular fill and rock mattresses; installation of Rock Bags in the gap that has formed between two of the sheet pile campshed retaining walls at the western end of the site; and reinstatement of the bed level behind the installed rock bags to match the top level of the sheet piles.

This application relates to the existing foreshore and campshed within the River Thames located adjacent to Swan Lane and Riverbank House. The site is accessed via an area of river walk off Swan Lane.

The wider site, originally Swan Lane Pier, fell out of use in 2012 but retains two existing dolphins and a singular mooring pile riverside, bankseat, steps, loading bay and associated services landside. Swan Lane is located on the north bank of the River Thames and lies upstream of London Bridge and downstream of Cannon Street rail

bridge. The campshed itself covers an area of approximately 2,000sqm of the foreshore, extending approximately 16.8m out from the river wall at its widest point.

The site is not listed or within the curtilage of a listed building, although Fishmongers Hall (Grade II – 56m) and the towers of Cannon Street Railway Station (Grade II – 64m) are within close proximity to the application site. The site is not within a conservation area.

Historically, timber piles have been used to maintain a raised bed profile and support flood defence in this area. However, repair and refurbishment works are now required, as part of the campshed has slumped and several of the timber piles have failed, resulting in bed levels dropping. This in turn, has compromised the structural integrity of the river wall, opening a crack in the wall through water erosion. The proposed works are to essentially repair the existing river defence in the foreshore area.

In principle, the proposal is considered to sustain the river's functional uses in transport, navigation and recreation, whilst minimising risks to the City's communities from flooding. As such the proposal is considered to be acceptable in principle, according with the NPPF, London Plan Policy SI 14 AND SI 17, Local Plan Strategic Policy CS9 and draft City Plan 2040 Strategic Policy S17.

61 representations have been received from consultation and all object to the proposals. The representations covered the broad themes of (a) impact on archaeological / historic matters; (b) noise and light pollution; and (c) impact on natural habitats. The representations are addressed within the body of the attached report.

In design terms the proposal would be in keeping with the surrounding foreshore character and the works would read as a continuation of the existing surrounding campshed. Furthermore, the proposal would not have any detrimental harm on the importance or setting of the surrounding designated heritage assets in compliance with CS12 and DM12.1 of the Local Plan 2015 and S11 and HE1 of the emerging City Plan 2040.

The site does not lie within a Designated Archaeological Area, however as the site is located within the historic core of the City of London an Archaeological Desk-Based Assessment has been submitted in support of the application. The Assessment concludes that there is likely a low potential for significant archaeology in the area, however a further written scheme of investigation (WSI) ensuring a programme and methodology of site investigation and recording is suggested prior to development. This conclusion is accepted by Historic England and GLAAS and as such a condition is recommended requiring the submission of such a programme.

Ecologically the application would have an acceptable impact in line with policy and the proposal is also considered to have an acceptable impact on Transport, Highways and Air Quality given the modest nature of the proposal.

Finally, the surrounding neighbouring properties would not be unduly impacted given the location and scale of the works.

Overall, the proposals are considered to accord with the development plan. When taking all matters into consideration, subject to the recommendations of this report it is recommended that planning permission be granted.

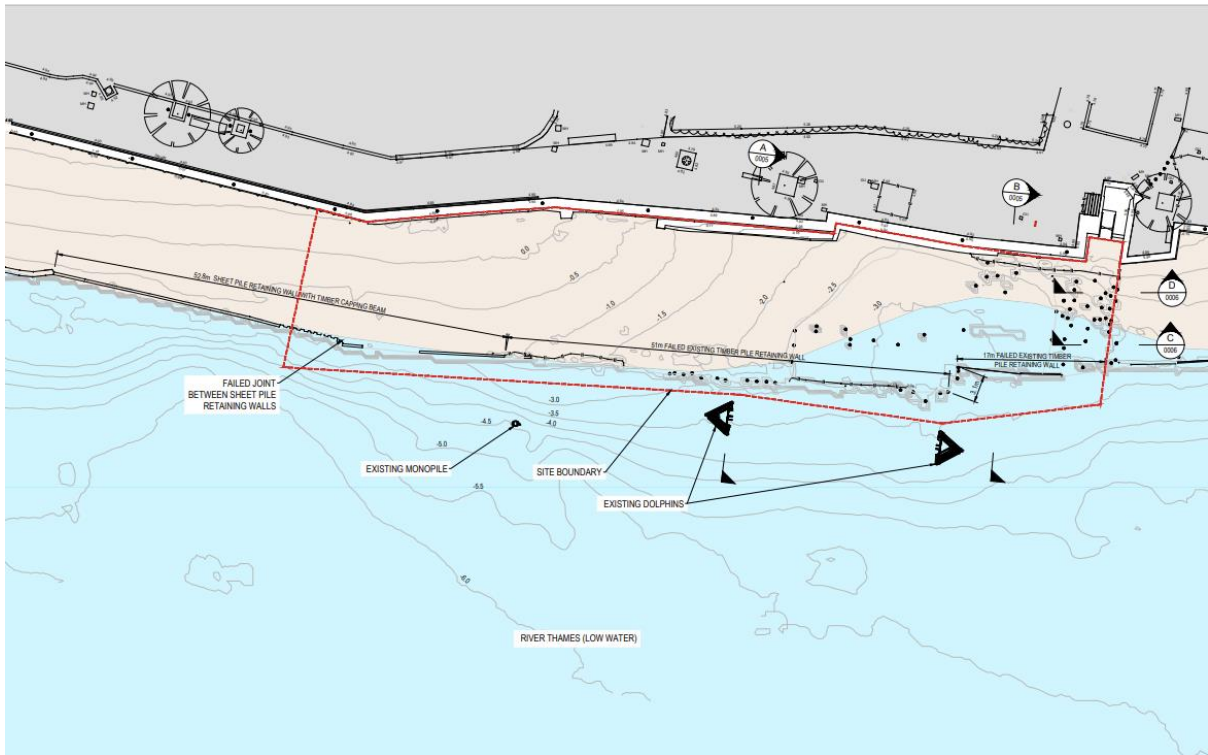
Applying the approach in section 38(6) of the Planning and Compulsory Purchase Act 2004, it is considered that the proposed development complies with the development plan as a whole. Other material considerations also support the grant of planning permission. Officers recommend that planning permission should be granted for the proposed development subject to all the relevant conditions being applied.

Recommendation

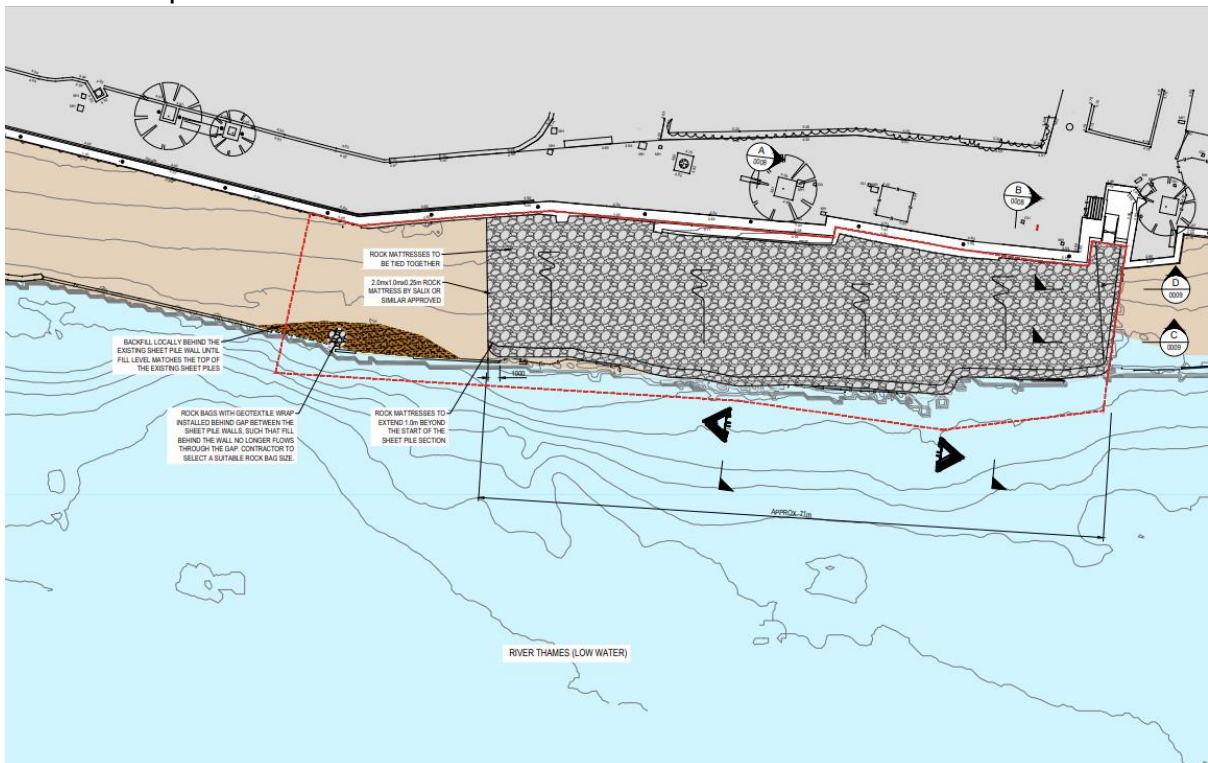
- (1) That Planning Permission be granted for the above proposal in accordance with the details set out in the attached schedule.

Plans

Plan 1: Existing Site Plan



Plan 2: Proposed Site Plan



Photos

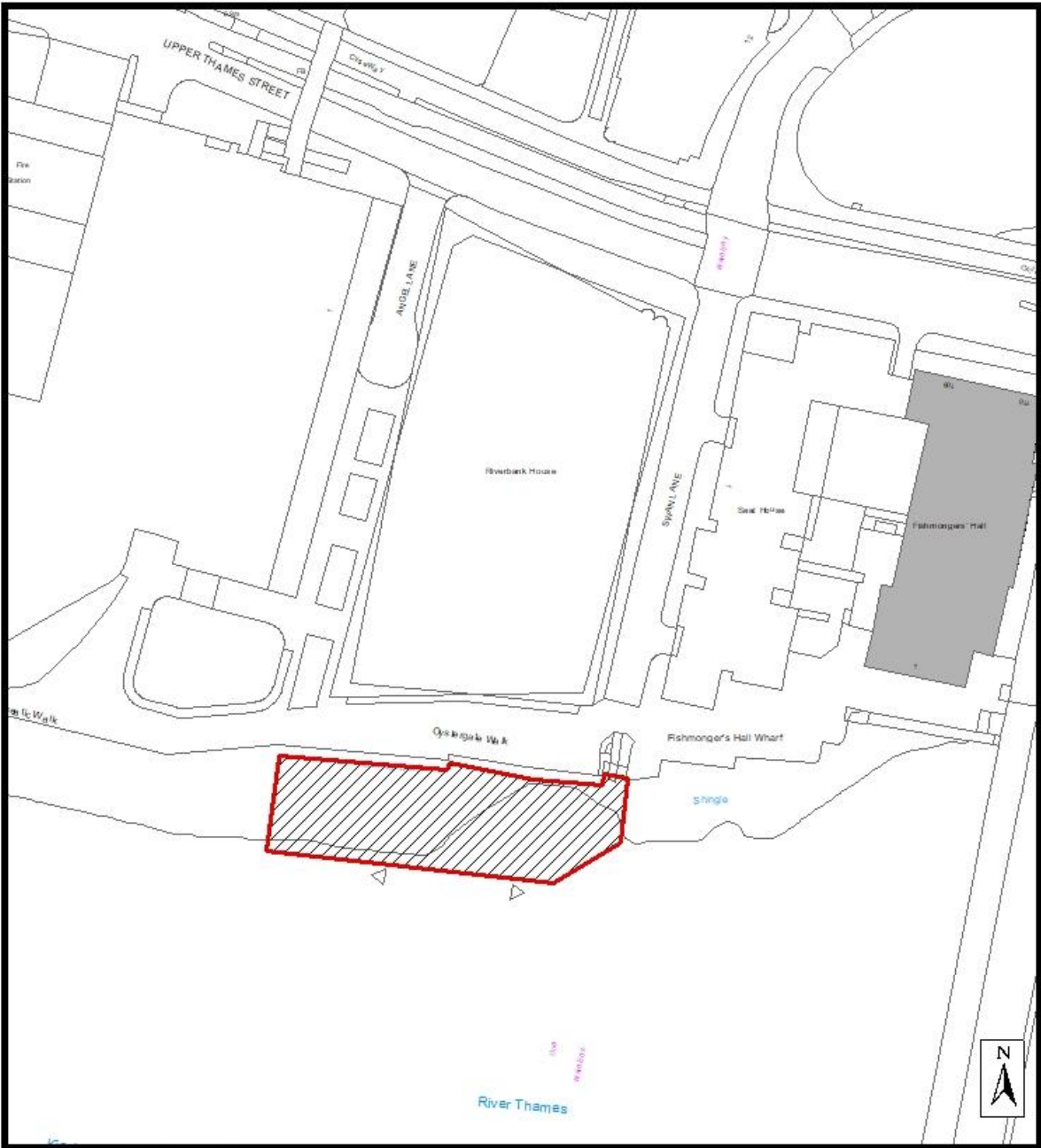
Photo 1: Site as of 2016



Photo 2: Site as of 2022






Site Location Application Plan



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ADDRESS:
Swan Lane - River Foreshore

CASE No.
24/00938/FULLR3

-  SITE LOCATION
-  LISTED BUILDINGS
-  CONSERVATION AREA BOUNDARY
-  CITY OF LONDON BOUNDARY



ENVIRONMENT DEPARTMENT

Main Report

City of London Application and Regulation 64(2)

1. Regulation 64(2) of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 (“the EIA Regulations”) requires that where a proposal for EIA development is brought forward by the City of London Corporation and the City of London Corporation will also be responsible for determining that application for planning permission, they must make appropriate administrative arrangements to ensure that there is a functional separation, when performing any duty under the EIA Regulations, between the persons bringing forward the proposal for development and the persons responsible for determining that proposal.
2. A handling note in line with Regulation 64(2) of the Town and Country Planning (Environmental Impact Assessment) Regulations (2017) has been produced alongside this application, in line with procedural requirements related to the City of London acting as both Local Planning Authority and as Applicant. Further context is provided within the note as detailed within the background papers.

Site and Surroundings

3. This application relates to various works to the existing campshed within the River Thames located adjacent to Swan Lane, Oystergate Walk and Riverbank House. The site is accessed via an area of river walk off Swan Lane.
4. The site, originally Swan Lane Pier, fell out of use in 2012 but retains two existing dolphins and a singular mooring pile riverside and bankseat, steps, loading bay and associated services landside. Swan Lane is located on the north bank of the River Thames and lies upstream of London Bridge and downstream of Cannon Street rail bridge. The campshed itself covers an area of approximately 2,000sqm of the foreshore, extending approximately 16.8m out from the river wall at its widest point.
5. The immediate surrounding area is a mix of office, commercial buildings, river frontage, road and rail infrastructure (bridges) and the river itself. Immediately adjacent are several office buildings including Riverbank House, Nomura Bank (1 Angel Lane) and Fishmonger’s Hall.
6. Fronting onto the south side of the river in LB Southwark, approximately 200m from the site, there are various office blocks and former wharf buildings which are now converted for either office or residential use. A replica of Sir Francis Drake’s ship, the Golden Hinde, is located in between two buildings in the St. Mary Overie dock.

7. The site is not listed or within the curtilage of a listed building, although Fishmongers Hall (Grade II – 56m) and the towers of Cannon Street Railway Station (Grade II – 64m) are within close proximity to the application site. The site is not within a conservation area.
8. The application site falls within the Thames Policy Area as designated by the London Plan (2021) and within a Site of Metropolitan Importance for Nature.

Site Planning History

9. The planning records below represent those most relevant to the proposed development:
10. Planning permission was granted in 1989 (Reference: 2899/1D) to position or to moor adjacent to existing pontoon barge an additional swim ended pontoon measuring 200 ft.
11. Planning permission was granted in 1985 (Reference: 2899/1C) to position a Livery Barge on Old Swan Pier (as a replacement for P.S. 'Princess Elizabeth' a proposed floating restaurant & function venue).
12. In 1975 planning permission was granted (Reference: 2899/1A) to permanently moor Paddle Steamer "Princess Elizabeth" with a brow from Watermen's Steps, to be used for restaurant purposes, at Watermen's Steps, Swan Lane.
13. Planning permission was refused in 1972 (Reference: 2899/1) for a landing platform adjoining Swan Lane Car Park for mooring of helicopter.
14. Planning permission was refused in 2020 (Reference 19/00116/FULL) for the Erection of a new pier within the River Thames at Swan Lane, to comprise a refurbished landside access platform; new canting brow and pontoon; dredging and filling of river bed; repair and reinstatement of campshed and riverbank; replacement of mooring pile and installation of additional mooring pile. (Reason for refusals relating to (1) use of the pier and (2) impact on surrounding occupiers).

Background of Application

15. The Applicant (City of London) have confirmed that the Environment Agency contacted the City in January 2019 to state that urgent repairs were needed to the foreshore. The foreshore has already eroded more than expected in the last two years and it is foreseeable that the erosion could progress so far that the toe of the river wall is exposed.

16. It is likely that emergency stabilisation works will then be required at this point, after which the main repairs works would also still need to be completed. Officers note that these works (including the works detailed under this application) could be undertaken by the Environmental Agency without the need for planning permission under the Floods and Water Management Act 2010 should the situation worsen.
17. For clarity campsheds are manmade raised areas of river foreshore that were typically faced with timber piles. Campshedding is a means to provide structural support to reduce scour and prevent the riverbank collapsing. Historically, campsheds were also constructed to provide a level platform on which boats would sit at low tide keeping them upright.

Proposed Development

18. Planning permission is sought for the proposed removal/cutting down to bed level of the existing campshed timbers; removal of gabion baskets and steel tie rods between the existing campshed and anchor posts; installation of new sloped revetment formed from imported granular fill and rock mattresses; installation of Rock Bags in the gap that has formed between two of the sheet pile campshed retaining walls at the western end of the site; and reinstatement of the bed level behind the installed rock bags to match the top level of the sheet piles
19. Historically, timber piles have been used to maintain a raised bed profile and support flood defence in this area. However, repair and refurbishment works are now required, as part of the timber campshed has slumped and several of the timber piles have failed, resulting in bed levels dropping. This in turn, has compromised the structural integrity of the river wall, creating a crack. The proposed works are to essentially repair the existing river defence.
20. The works encompasses an area of approximately 2,000sqm (0.2 ha) of the foreshore. Where the existing timber piles have failed, these are proposed be cut down to bed level and the pieces removed from site. The riverbed is then proposed to be reprofiled using approximately 1,300m³ of 6A granular fill (e.g. natural gravel, natural sand, crushed gravel, crushed rock etc) to essentially restore the bed up to historic campshed bed levels. To form a toe of the sloped revetment aqua rock bags would be placed along the same line of the existing failed timber piles. The granular fill would be backfilled behind the rock bag toe and a geotextile layer would be laid to create a firm base on which 2m x 1m x 0.25m rock mattresses would then be laid. The proposed rock mattress would restore approximately 71m of campshed. Well-graded class 6A granular fill material is then proposed be laid above the rock mattresses

where possible, to maximise the loose material at the surface and provide a more biodiverse design and natural appearance.

21. Furthermore, there is a gap that has opened in between two of the existing sheet-piled retaining walls at the western end of the campshed and it is proposed to fill this with rock bags and local fill material.
22. Only those lengths of the timber revetment that have failed will be repaired. There are no plans to replace the entire revetment.
23. The proposed works do not include or facilitate future vessel mooring or consist of any alteration to the use or function of the historical pier.

Consultation

Statutory Consultation

24. As part of the current application, the City of London Corporation acting as the Local Planning Authority ('LPA') has undertaken consultation with neighbouring residents and other stakeholders in line with statutory duties. Statutory Consultee responses are provided below (summarised).
25. Environment Agency: No objection subject to informative.
26. Port of London Authority: - Prior to any works taking place a River Works Licence (RWL) will be required with the PLA. This requirement must be added as an informative as part of any forthcoming planning permission. For information the applicant is in contact with the PLA's Statutory Consents and Compliance Team with regard to obtaining a RWL. The PLA have no further comments to make on the planning application.

[Officer Note: The Applicant has confirmed that notice, with a description of the works and drawings was sent to the PLA on 28/08/24].

27. Network Rail: No objection subject to informative.
28. Historic England: supports the application on heritage grounds.
29. GLAAS (Historic England Archaeology): No objection subject to condition requiring a Written Scheme of Investigation (WSI).
30. LB Southwark: No comment.
31. City of London Air Quality Officer: No objection subject to conditions requiring the contractor to sign up to the Non-Road Mobile Machinery Register.

32. City of London Environmental Health Officer: No objection subject to conditions requiring a scheme to protect neighbours during construction and the requirement for the contractor to sign up to the Non-Road Mobile Machinery Register.
33. Copies of the representations are attached in full to this report.

Neighbour/Third-Party Consultation Responses

34. The planning application was advertised on site (23rd September 2024) and in the press (1st November 2024). Neighbouring occupiers located in the immediate vicinity at Fishmongers' Hall (Steward's Flat, Clerk's Flat) were consulted on an individual basis (23rd September 2024).
35. 61 representations have been received from the public consultation all of which object to the proposals. These include representation from individual members of the public as well as the River Residents Group and Society of Thames Mudlarks & Antiquarians.
36. Copies of the representations are attached in full to this report.
37. The representations covered the broad themes of (a) impact on archaeological / historic matters; (b) noise and light pollution; and (c) impact on natural habitats.
38. The table below summarises the public representations received:

Principal Themes	Nature of representations (Sample)	Number of comments on this theme	Response
Impact on Archaeological and Historic matters	Example 1 (extract): This area is of significant archaeological importance. Developing on this site would cause a huge loss of artifacts currently being found at this site of the foreshore which are important to understanding London's history and public knowledge.	61	An Archaeological Desk Based Assessment has been submitted to accompany the application and Historic England are satisfied with the outcome subject to a condition.

	<p>2. The narrow alley ways would become a thoroughfare and a place to loiter, for the venue's customers which would make it unbearable for the residents of Bolt Court given the layout of the courtyard. Such a venue should not be located near residential buildings.</p> <p>Example 2 (extract): I object to the destruction of this historical site known as Rome without further research and investigation.</p> <p>Example 3 (extract): This MUST NOT go ahead. This particular spot is of archaeological importance and is a sacred site of Roman and many other historical relics at this area in particular. Doing so would be losing history forever and also damaging the foreshore beyond repair. There is already signs that it is eroding and this will hasten the issue. Please think this through properly. It isn't wanted or needed when there are so many other places for people to enjoy themselves. Please don't ruin the environment. Thank you for your time.</p>		<p>Please refer to the Archaeology section of the report (Para: 72).</p> <p>The example responses provided also elude the potential use of the pier (i.e. for the mooring of a vessel) however this application solely relates to works to the river foreshore. These elements of the responses are therefore not relevant to the application.</p>
Noise and Light Pollution	Example 1 (extract): Object to the noise, the light pollution and because it's an archaeological site that must	2	An Intertidal Phase 1 Biotope Survey and Water Framework Directive Assessment

	<p>be protected. The foreshore is a sensitive ecological and historic site and this would be detrimental to its survival.</p>		<p>have been submitted in support of the application. A proposed Construction Environment Management Plan is proposed to be secured (Condition 5) to secure appropriate construction methods and prevent impact on ecology.</p>
Impact on Natural Habitats	<p>Example 1 (extract): ...Suggested works from Planning Application detail would cause irreversible damage to the natural fabric and habitats of the foreshore which is significantly historically important and environmentally sensitive.</p> <p>There are a number of significant historical finds that have been recorded by the Finds Liaison Officer (Stuart Wyatt) from this stretch 'Roman' yielding foreshore which cannot be ignored and once disturbed will be lost forever.</p> <p>Vast parts of the foreshore and old barge beds have been continually blown out along the course of the banks -both North and South due to the Thames Clippers persistently being used in the river, such Craft were used in Sydney Harbour until they were banned due to the</p>	3	<p>As above. An Intertidal Phase 1 Biotope Survey and Water Framework Directive Assessment have been submitted in support of the application. A proposed Construction Environment Management Plan is proposed to be secured (Condition 5) to secure appropriate construction methods and prevent impact on ecology.</p> <p>Similar to the above the example responses provided also elude the potential use of the pier (i.e. for the mooring of a vessel) however this application solely relates to works to the river foreshore. These</p>

	<p>environmentally and ecological damaged being caused by their wakes tearing the the Harbour bed to shreds, more damage is being caused by them day by day and as a PLA registered Thames Foreshore Permit Holding Mudlark I, plus hundreds more can vouch for seeing this continued destruction along the Thames foreshore.</p> <p>Until you put a stop the cause of this damage from continually happening it is pointless trying to haphazardly carrying out 'quick fixes' that have not been properly consulted by the correct authorities which is not solely governed by the Corporation of London who's interest in this is to put back a walkway down to an intended mooring for a boat to be permanently moored as another unwanted Drinks/Party/Club for which the Corporation of London can claim licences to and have other funds made available to themselves...</p>		<p>elements of the responses are therefore not relevant to the application.</p>
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Policy Context

- 39. The Development Plan consists of the London Plan 2021 and the City of London Local Plan 2015. The London Plan and Local Plan policies that are most relevant to the consideration of this case are set out in Appendix B to this report.
- 40. The City of London (CoL) has prepared a new Local Plan, the City Plan 2040, which was published for Regulation 19 consultation in the Spring of 2024 and

submitted to the Secretary of State in late Summer 2024. Emerging policies are considered to be a material consideration with limited weight with an increasing degree of weight as the City Plan progresses towards adoption, in accordance with paragraph 48 of the NPPF. The emerging City Plan 2040 policies that are most relevant to the consideration of this case are set out in Appendix B to this report.

41. Government Guidance is contained in the National Planning Policy Framework (NPPF) December 2024 and the Planning Practice Guidance (PPG) which is amended from time to time.
42. The Historic England Good Practice Advice notes, including Note 3 The Setting of Heritage Assets and Note 2 Managing Significance in Decision-Taking in the Historic Environment.
43. Relevant City Corporation Guidance and SPDs includes the Thames Strategy Supplementary Planning Document (City of London, 2015).

Policy Considerations

44. The Corporation, in determining the planning application has the following main statutory duties:-
 - to have regard to the provisions of the development plan, so far as material to the application, local finance considerations so far as material to the application, and to any other material considerations (Section 70 Town & Country Planning Act 1990); and
 - to determine the application in accordance with the development plan unless material considerations indicate otherwise (Section 38(6) of the Planning and Compulsory Purchase Act 2004).
45. The National Planning Policy Framework (NPPF) states at paragraph 2 that “Planning Law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise”.
46. The NPPF (2024) states at paragraph 8 that achieving sustainable development has three overarching objectives, being economic, social, and environmental.
47. Paragraph 10 of the NPPF states that “at the heart of the Framework is a presumption in favour of sustainable development. That presumption is set out at paragraph 11. For decision-taking this means:
 - a) approving development proposals that accord with an up-to-date development plan without delay; or

- b) where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, granting permission unless:
 - (i) the application of policies in this Framework that protect areas or assets of particular importance provides a strong reason for refusing the development proposed; or
 - ii) any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole, having particular regard to key policies for directing development to sustainable locations, making effective use of land, securing well-designed places and providing affordable homes, individually or in combination.
48. Paragraph 49 states that local planning authorities may give weight to relevant policies in emerging plans according to:
- a) the stage of preparation of the emerging plan (the more advanced its preparation the greater the weight that may be given);
 - b) the extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
 - c) the degree of consistency of the relevant policies in the emerging plan to this Framework (the closer the policies in the emerging plan to the policies in the Framework, the greater the weight that may be given).
49. Chapter 8 of the NPPF seeks to promote healthy, inclusive, and safe places.
50. Chapter 12 of the NPPF seeks to achieve well designed places. It advises that “The creation of high quality and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities.”
51. Chapter 15 of the NPPF seeks to conserve and enhance the natural environment. It advises that planning policies and decisions should contribute to and enhance the natural and local environment by protecting and enhancing valued landscapes, sites of biodiversity or geological value and soils (in a manner commensurate with their statutory status or identified quality in the development plan).
52. Chapter 16 of the NPPF relates to conserving and enhancing the historic environment, it advises that Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage

asset) taking account of the available evidence and any necessary expertise. They should take this into account when considering the impact of a proposal on a heritage asset, to avoid or minimise any conflict between the heritage asset's conservation and any aspect of the proposal.

53. It goes on to advise, "In determining applications, local planning authorities should take account of:
- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
 - b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- the desirability of new development making a positive contribution to local character and distinctiveness."

Main Considerations of Application

54. In considering the application for planning permission account has to be taken of the statutory and policy framework, the documentation accompanying the application, and the views of both statutory and non-statutory consultees.
55. The principal considerations in this case are:
- The extent to which the proposals comply with the development plan;
 - The extent to which the proposals comply with the NPPF;
 - The principle of the development;
 - The impact of the development in design and heritage terms (including indirect) to the setting of surrounding heritage assets;
 - The impact of the proposal in terms of archaeology;
 - The transport and highway impacts of the proposal;
 - The impact of the proposed development on the amenity of residential occupiers, both within and adjacent to the proposed development;
 - The ecological impacts of the proposal;
 - The impact on the development on flood risk; and
 - Consideration towards impacts upon Human Rights and Equality.

Principle of Development

56. London Plan Policy SI 14 (Waterways) sets out the strategic role and importance of waterways in London. The policy states that Development Plans and development proposals should address the strategic importance of London's network of linked waterways, including the River Thames, and should seek to maximise their multifunctional social, economic and environmental benefits. The Policy also requires that to reflect the distinctiveness of areas that specifically relate to the River Thames, relevant

Development Plans should designate, and ensure the maintenance of, Thames Policy Areas (TPAs).

57. Policy SI 17 (Protecting and enhancing London's waterways) of the London Plan sets out that development proposals that facilitate river restoration, including protecting and improving the foreshore and floodplain should be supported.
58. Local Plan Strategic Policy CS9 (Thames and the Riverside) seeks to ensure that the City capitalises on its unique riverside location, sustaining the river's functional uses in transport, navigation and recreation, whilst minimising risks to the City's communities from flooding.
59. Draft City Plan 2040 Strategic Policy S17 (Thames Policy Area) echoes the above Local Plan Strategic Policy CS9.
60. The proposed development seeks alterations to the foreshore of the River Thames. The proposed works are essentially seeking to restore and enhance the campshed to its historic level, while preventing future substantial damage to the river wall. The proposal would therefore be minor in nature, represent a development that would facilitate the restoration of the foreshore and align with the general requirements of the above policies.
61. The proposal would sustain the river's functional uses in transport, navigation and recreation, whilst minimising risks to the City's communities from flooding. As such the proposal is considered to be acceptable in principle, according with the NPPF, London Plan Policies SI 14 and SI 17, Local Plan Strategic Policy CS9 and draft City Plan 2040 Strategic Policy S17.

Design

62. The relevant local policies for consideration are CS10, DM10.1, DM10.4, CS12, DM12.1 of the Local Plan policies and S8, DE2 of the emerging City Plan, and London Plan policies D4.
63. Local Plan Core Strategic Policy CS10 (Design) seeks to promote a high standard of design and sustainable buildings, streets and spaces, having regard to their surroundings and the historic and local character of the City and creating an inclusive and attractive environment. Policy DM10.1 requires all developments to be of a high standard of design and to avoid harm to the townscape and public realm. These requirements are echoed in policies and S8, DE2 of the emerging City Plan.

64. The proposed works seek to physically alter the appearance of the eroded campshed and return it to historic campshed levels. The proposal would not introduce any new significant physical massing apart from the proposed rock mattress, overlaid with well-graded class 6A granular fill material (natural gravel, natural sand, crushed gravel, crushed rock etc), which would read as a continuation of the existing surrounding campshed. The semi-naturalised appearance of these materials would ensure that the proposal would read as typical river-side feature and over time this would soften as the development becomes more established within its setting.
65. The proposal would be in keeping with the surrounding foreshore character and the works would be acquiescent with the existing pier structure and river flood wall. The proposal is therefore considered to be appropriate in relation to its surroundings and have due regard to Local Policy.
66. The proposed development is therefore considered to comply with CS10, DM10.1, DM10.4, CS12, DM12.1 of the Local Plan policies and S8, DE2, of the emerging City Plan, and London Plan policies D4.

Heritage and Strategic Views

Impact on Setting of Listed Buildings

67. The application site is not within any national or internationally designated sites of heritage conservation importance. There are several listed buildings located within the immediate vicinity of the site. The Grade II* Fishmonger's Hall is located approximately 56m to the northeast and the eastern tower of the twin Grade II towers at Cannon Street Station, is approximately 64m to the west.
68. The proposed development is minor in nature and located away from these listed buildings on the River Thames foreshore which would, at various times, be submerged. The proposal would not block or obscure the view of the towers or Fishmonger's Hall; or detract from their pre-eminence and setting on this part of the Thames. The proposal, therefore, is not considered to cause any harm or impact to the significance or setting of the listed buildings.

The London View Management Framework (LVMF)

69. For completeness, the proposal has been considered in relation to the LVMF and other Strategic Views (including the World Heritage Site).
70. The campshed falls within View 11A (London Bridge), from where there are views upstream of the bridge towards St Paul's Cathedral and the listed towers of Cannon Street station. From Viewpoint 11A, the campshed would

be seen at low tide but is to the side of the main view towards St Paul's. As a semi-naturalised feature within the riverbed, any changes are not considered to be significant in the context of the views from London Bridge. This is particularly pertinent as the campshed itself is below the waterline for large periods.

71. The proposal is considered to accord with the guidance for this view (para 195 and 196 of the LVMF) in the manner in which it does not block or impair views of landmarks including St Paul's Cathedral or Cannon Street Station Towers.

Conclusion on Heritage

72. The proposal would preserve the settings and significance of all relevant designated or non-designated heritage assets and would accord with policies CS12 (1) and DM12.1 (1) of the Local Plan 2015 and S11 (2) and HE1 of the emerging City Plan 2040.

Archaeology

73. Section 16 of the NPPF and Policy HC1 of the London Plan require the conservation of archaeological interest as part of the planning process. Paragraph 200 of the NPPF obliges applicants to provide an archaeological assessment when development may affect heritage assets of archaeological significance.
74. Local Plan Core Strategic Policy CS12 (Historic Environment) requires developments to conserve or enhance the significance of the City's heritage assets and their settings by protecting and promoting the evaluation and assessment of the City's ancient monuments and archaeological remains and their settings, including the interpretation and publication of results of archaeological investigations. Policy DM12.4 (Ancient monuments and archaeology) goes on to require planning applications which involve excavation or ground works on sites of archaeological potential to be accompanied by an archaeological assessment and evaluation of the site, including the impact of the proposed development. These requirements are echoed within policies S11 and HE2 of the Draft City Plan 2040.
75. London Plan Policy HC1 (Heritage conservation and growth) requires development proposals should identify assets of archaeological significance and use this information to avoid harm or minimise it through design and appropriate mitigation.

76. The application site does not contain any Scheduled Ancient Monuments and does not lie within a Designated Archaeological Area as defined in the Scheduled Ancient Monuments & Archaeological Areas Act 1979. The proposed development site is within the historic core of the City of London, an area recognized for its significant archaeological potential and as such an Archaeological Desk-Based Assessment has been submitted in support of the proposals.
77. The assessment states that the application site lies within the course of the River Thames, between high and low water. The site has been part of a dynamic hydrological system since the last Ice Age and has never been dry land. Until comparatively recently, the 16th century, the site was always submerged.
78. The subject site lay deep in the river until the late mediaeval period when the advancing waterfronts meant the site became part of the intertidal zone. It would have been crossed by numerous short lived timber structures, such as fish traps, jetties, mooring posts, gantries and other shipping aids which were recorded in a 2019 walk-over survey of the site.
79. The Archaeological Desk-Based Assessment therefore concludes that there is a low potential for significant archaeology in the area. The majority of potential archaeology is likely to be items such as timber structures, such as fish traps, jetties, mooring posts, gantries and other shipping aids from the 16th century. Furthermore, the proposed works would not entail substantial ground disturbances as it is proposed that any imported fill would effectively be sealed by a proposed rock mattress.
80. The Greater London Archaeological Advisory Service (GLAAS) have reviewed the submitted assessment and accepted the outcome of the documentation subject to a condition requiring the submission of a written scheme of investigation (WSI) prior to any development taking place. This WSI would include a programme and methodology of site investigation and recording; a programme and methodology for an archaeological foreshore survey; and a programme for post-investigation assessment and subsequent analysis, publication & dissemination and deposition of resulting material.
81. This WSI would therefore essentially safeguard the archaeological interest on this site and provide clarity on what investigations are required, and their timing in relation to the development programme.
82. It is noted that the majority of responses received from the third-party consultation process are specifically regarding the archaeology potential of the site and in particular the potential for Roman artifacts. While excavations

to the north of the waterfronts have revealed a significant number of lavish and elaborate Roman masonry buildings which would have dominated the approach to Londinium from the Thames, the foreshore itself site lay within the deep waters at the middle of the Thames during the Roman period. The foreshore has also now somewhat eroded from its former prevalence with a large amount of the foreshore now dissipated into the Thames (hence the basis for the application). The proposed works seek to protect the foreshore from further erosion, preventing the loss of existing foreshore and restore the historic campshed bed level. Notwithstanding this however, the proposed condition would ensure appropriate investigation of the remaining foreshore area and as such considered acceptable by Officers, GLAAS and Historic England.

83. The proposed development is therefore considered to comply with relevant policies, including Local Plan DM12.4, Draft City Plan 2040 HE2, and London Plan HC1, subject to the recommended archaeological condition.

Transport, Highways and Air Quality

84. Policy DM16.1 of the Local Plan 2015 states that development proposals which are likely to have effects on transport must be accompanied by an assessment of the transport implications during both construction and operation in particular addressing impacts on: road dangers; pedestrian environment and movement; cycling infrastructure provision; public transport; and the street network.
85. The proposed development relates to remedial works to the River Thames foreshore and such the transport implications of the completed development would be negligible given the nature of the application (i.e. there is no requirement for parking/cycle parking/post construction servicing).
86. In terms of potential works during construction, works are expected to be carried out from the river using barge-mounted equipment (crane, 360° excavator). These works would need to be undertaken at low tide to enable full access and to minimise disturbance to the riverbed and dispersion of sediments. The submitted documentation states that where required, personnel may be required onto the foreshore if it is deemed safe to do so; however, this is likely to be limited. The details of this would be secured through a condition requiring the submission of a Construction Environmental Management Plan (CEMP).
87. Subject to the proposed mitigation measures, which would be secured by condition, the proposal is considered to be in line with DM16.1 of the Local Plan 2015.

88. With regard to Air Quality, Local Plan Policy CS15 requires developments to positively address air quality concerns. The Draft City Plan 2040 (Policy DE1) mandates that developments meet London Plan requirements for carbon emissions and air quality, while Policy HL2 requires developments to be at least Air Quality Neutral, encouraging the use of non-combustion technologies. London Plan Policy SI1 focuses on improving air quality across London, with an emphasis on Air Quality Neutrality and Positive contributions.
89. The City's Air Quality Officer has reviewed the proposed development and is satisfied with the proposal providing that during construction the applicant follows best practices to limit emissions from Non-Road Mobile Machinery (NRMM).
90. With the recommended conditions and mitigation measures, the proposed development is expected to have minimal negative impact on local air quality. The scheme complies with Local Plan Policy CS15, Draft City Plan 2040 (Policies DE1 and HL2), and London Plan Policy SI1.

Impact on Residential Amenity

91. Local Plan Policies CS21 (Housing) and DM21.3 ('Residential Environment') and draft City Plan policies S3 and HS3, requires amenity of existing residents in identified residential areas to be protected.
92. Local Plan policy DM15.7 and Draft City Plan policy HL3 require noise pollution to be considered.
93. Local Plan policy DM10.7, draft City Plan policy DE8, and London Plan policy D6 considers impact of development on existing daylight and sunlight of residential properties.
94. The proposed development represents remedial works to the foreshore and as such does not propose significant structures or have residential amenity implications. The works would be located on the foreshore and shielded from the properties on Oystergate Walk from the existing river wall.
95. The nearest residential properties, located in at Fishmongers' Hall (Steward's Flat, Clerk's Flat) would not be impacted unduly by the development given the separation distance (55m+) and minor nature of the works. Equally the properties located in LB Southwark on the southern side of the river are located some 200m from the development and as such would not be impacted unduly.

96. While, with every development, there may be noise from construction given the minor nature of works this is not considered to be significant and would be for a temporary period. With that being said, given the potential noise/disturbance a condition is proposed requiring the submission of details of a scheme to protect surrounding properties during the construction phase. The proposal is therefore considered to accord with the Local Plan and Draft City Plan policies in this regard.

Ecological Impacts and Biodiversity Net Gain

97. Ecological Impacts

98. Chapter 15 of the NPPF relates to preserving and enhancing the natural environment. The protection and enhancement of the environment is one of the three overarching objectives that define sustainable development.
99. Policy CS15 of the adopted City Plan (2015) paragraph 4(vi) states the need to enhance biodiversity and provide for its conservation and enhancement, particularly for the City's flagship species and the City's priority habitats. Policy OS3 of the draft City Plan 2040 requires development to incorporate measures to enhance biodiversity, including measures recommended in the City of London Biodiversity Action Plan (BAP, 2021) in relation to particular species or habitats and action plans.
100. Policy DM19.2 (Biodiversity and urban greening) of the adopted City Plan (2015) states that developments should promote biodiversity and contribute to urban greening as well as identifying several Sites of Importance for Nature Conservation (SINCs).
101. The application site is not located within any national/international designated sites for ecological value, although locally the site is designated within a Site of Metropolitan Importance to Nature Conservation (SINCs) and the closest Local Nature Reserve is Russia Dock Woodland located 3.36km away from the site.
102. Sites of Importance to Nature Conservation (SINCs) are locally designated areas which are recognised as being of particular importance to wildlife and biodiversity. Although a non-statutory designation, SINCs are afforded a high level of protection within the planning system. Sites of 'Metropolitan Importance' are designated by London Plan Policy G6 and are considered to contain the best examples of London's habitats.
103. An intertidal Phase 1 and Preliminary Ecological Appraisal, incorporating UK Habitat Classification, of the Riverbank House Campshed has been submitted in support of the application.

104. The assessment identified three biotopes (“Barren littoral coarse sand”, “Littoral mixed sediments “Littoral mixed sediment” and “Artificial littoral coarse sediment”) in the site, none of which qualify for legal habitat protection.
105. Furthermore, the report confirms that there are records of nesting Lesser black-backed gull (*Larus fuscus*), an Amber listed in Birds of Conservation Concern 4, located 0.089km from the site. Therefore, it is recommended that any works involving deconstruction of the existing campshed retaining wall should either commence outside of the breeding bird season (March-August, inclusive) or have an on-site ecologist check for nests. A condition is proposed to ensure a Construction Environmental Management Plan (CEMP) is submitted to secure the above requirements.
106. In terms of the impact on the Thames as an actual body of surface water, a Water Framework Directive Assessment has been submitted in support of the application. The document states that the works would result in small-scale local changes in flow, with negligible impacts in the main channel. The risk of sediment suspension, scour and accumulation is low and is proposed to be mitigated against through various biosecurity measures included in a CEMP to avoid the introduction or spread of invasive non-native species in the estuarine environment. This is essentially done by ensuring that the rock mattresses and fill material have been checked and treated using appropriate biosecurity measures prior to installation.
107. Policy G6 (Biodiversity and access to nature) states that where harm to a SINC is unavoidable, and where the benefits of the development proposal clearly outweigh the impacts on biodiversity, the following mitigation hierarchy should be applied to minimise development impacts: 1) avoid damaging the significant ecological features of the site 2) minimise the overall spatial impact and mitigate it by improving the quality or management of the rest of the site 3) deliver off-site compensation of better biodiversity value.
108. The proposal represents a repair to the existing flood defence for the Thames and wider London. The significant need for the work would provide wider benefit in terms of the repair and retention of the existing flood defence. Furthermore, the submitted documents confirm that no significant impacts to the SINC would be caused, and the proposal would not see the significant loss of specific habitat. Given the further conditions would secure this, the proposal is therefore considered to be consistent with the general requirements of Policy G6 of the London Plan.
109. The Environment Agency has reviewed the documentation and as such considered the proposal acceptable. An Informative is recommended regarding the need for a Flood Risk Activity Permit which sits outside of the Planning Act.
110. Biodiversity Net Gain
111. In accordance with the NPPF, local policy drivers and recent legislative changes, proposals are expected to provide measurable net gains in

biodiversity. These should aspire to a minimum of 10% net gain in biodiversity which should be evidenced through the submission. Applications should aim to quantify the predicted change in ecological value of the site in light of the proposed developments to assess compliance against local and national policy. The BNG mandate set out in the Environment Act 2021, states that a target of 10% net gain in biodiversity should be reached, and biodiversity value maximised on site.

112. The Applicant has submitted a Biodiversity Net Gain metric calculation showing that the 10% is to be reached on site. Proposed habitat creation includes the introduction of natural rock mattress and artificial rock pools on site which would assist in the delivery of ecological gains. Under these proposals the development stands to result in a net gain of 20.74% in ecological value associated with area-based habitats and as such Biodiversity Net Gain is therefore considered to be achieved on site in line with the statutory requirements of The Environment Act 2021.

113. As per the above act, if the proposed development includes 'significant' onsite enhancements, the application will need to describe how it will maintain and monitor the habitat enhancement within a Habitat Maintenance and Monitoring Plan. For the City of London significant enhancements are likely to include (but are not limited to) biodiverse green roofs, urban trees and intensive green roofs. The proposed development's biodiverse gains are essentially provided by the materiality that would be used (i.e. the rock mattress/intertidal sediment/etc) and as such is not considered to constitute a 'significant' enhancement. With that being said, a completed Landscape and Ecological Management Plan (LEMP) would be required for all non-significant onsite gains. A condition is therefore proposed securing the LEMP which would cover all relevant habitats within the Biodiversity Gain Metric and provide details of when and how the habitats are monitored, who is responsible and how and when the measures would be secured for at least 30 years.

114. Conclusion

115. Subject to the proposed mitigation measures, which would be secured by condition, the impact upon wildlife and ecology is considered to be acceptable, in line with the NPPF, Policy C15 of the City Plan, G6 of the London Plan, the City Biodiversity Action Plan and Policy OS3 of the emerging draft City Plan 2024.

Flooding and Flood Risk

116. Chapter 14 of the NPPF relates to meeting the challenge of climate change, flooding and coastal change. Inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk (whether existing or future).

117. Local Plan Policy DM 18.1 (Development in the City Flood Risk Area) states that where development is proposed within the City Flood Risk Area evidence must be presented to demonstrate that the site is suitable for the intended use, in accordance with Environment Agency and Lead Local Flood Authority advice.
118. The application site is located within Flood Zone 3b. Flood Zone 3b (Functional Floodplain) comprises of land where water has to flow or be stored in times of flood.
119. The application relates to the foreshore and flood wall repair works and as such essentially constitutes a restoration of an existing flood defence rather than a new development within the flood zone. The site would be submerged for long lengths of time and the without the works being carried out there is the potential for the significant weakening of the flood wall, which would be contrary to the general aim of the NPPF.
120. Furthermore, under the NPPF Guidance 'Flood control infrastructure' is defined as a water-compatible development. The guidance goes on state that within Flood Zone 3b only the water-compatible uses should be permitted in this zone. As such the proposal is considered to be an appropriate use within the flood zone. This is again particularly pertinent as the campshed itself is below the waterline for large periods.
121. On this basis, given the requirement to protect the existing flood defence, the proposal is therefore considered to be compliant with the NPPF, Local Plan policy CS18 and the London Plan policy SI 13.

Public Sector Equalities Duty

122. When considering proposed development, the Public Sector Equality Duty requires the City of London Corporation to consider how the determination of the application will affect people who are protected under the Equality Act 2010, including having due regard to the effects of the proposed development and any potential disadvantages suffered by people because of their protected characteristics.
123. Under the Act, a public authority must, in the exercise of its functions, have due regard to the need to:
- Eliminate discrimination, harassment and victimisation and any other conduct that is prohibited by or under this Act.

- Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it.
 - Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
124. The relevant protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.
125. Public authorities also need to have due regard to the need to eliminate unlawful discrimination against someone because of their marriage or civil partnership status.
126. It is the view of officers that a decision to grant permission, subject to appropriate conditions, would not disadvantage those who are protected under the Equality Act 2010.
127. In relation to policy GG1 of the London Plan, the proposals are considered to support and promote the creation of an inclusive London where all Londoners, regardless of their age, disability, gender, gender identity, marital status, religion, race, sexual orientation, social class, or whether they are pregnant or have children, can share in its prosperity, culture and community, minimising the barriers, challenges and inequalities they face.

Human Rights Act 1998

128. It is unlawful for the City, as a public authority, to act in a way which is incompatible with a Convention right (being the rights set out in the European Convention on Human Rights (“ECHR”).
129. Officers have given consideration towards the interference with the right to respect for one’s private and family life (Article 8 of the ECHR) or peaceful enjoyment of one’s possessions (Article 1 of Protocol 1), including by causing harm to the amenity of those living in nearby residential properties. Officers have assessed the level of harm that would result to neighbouring amenity to be acceptable subject to the recommended conditions, and therefore do not consider the proposal would result in an infringement of the ECHR as a result of the proposal.
130. Therefore, it is the view of officers that there would be no infringement of Article 8 or Article 1 of Protocol 1 of the ECHR as a result of refusal of planning permission.

Conclusions

131. The proposal has been assessed in accordance with the relevant statutory duties and having regard to the development plan and other relevant policies and guidance, SPDs and SPGs and relevant advice including the National Planning Policy Framework, and the emerging Local Plan and all other material considerations have been considered.
132. The proposed application seeks to remedy the current urgent issue of erosion to the foreshore in this area which is a requirement of the Environment Agency as further erosion could have significant impact to the existing river wall if left unmanaged.
133. The proposal is considered to sustain the river's functional uses in transport, navigation and recreation, whilst minimising risks to the City's communities from flooding and as such the proposal is considered to be acceptable in accordance with the NPPF, London Plan and draft City Plan 2040.
134. The appearance of the proposed development would be modest and for most of the time be submerged below the waterline. When viewable the proposal would not look out of character within the existing foreshore and have no harm upon surrounding heritage assets.
135. A proposed condition would secure an appropriate programme and methodology of future archaeological site investigations and recordings prior to any development taking place, in line with Historic England and GLAAS advice.
136. The impact upon residential amenity, transport, air quality, wildlife and ecology is considered to be acceptable, in line with relevant policies, subject to the recommended conditions.
137. Overall, therefore, the proposals are considered to accord with the development plan. When taking all other material considerations into account, subject to the recommendations of this report it is recommended that planning permission be granted.

APPENDIX A: BACKGROUND PAPERS

Plans titled:

Location Plan
2401-BRL-02-XX-DR-C-0001 Rev T01
2401-BRL-02-XX-DR-C-0002 Rev T01
2401-BRL-02-XX-DR-C-0003 Rev T01
2401-BRL-02-XX-DR-C-0004 Rev T01
2401-BRL-02-XX-DR-C-0004 Rev T01
2401-BRL-02-XX-DR-C-0006 Rev T01
2401-BRL-02-XX-DR-C-0007 Rev T01
2401-BRL-02-XX-DR-C-0009 Rev T01
2401-BRL-01-XX-SK-C-0001 Rev P01

Documents titled:

Archaeological Desk-Based Assessment (Mills Whipp Projects June 2024)
Intertidal Phase 1 Biotope Survey (Thomson Environmental Consultants July 2024)
Riverbank House campshed repairs Description of work (City of London – Applicant)
Water Framework Directive Assessment Riverbank Campshed (Thomson Environmental Consultants September 2024)
REGULATION 64(2) HANDLING NOTE (22 Jan 2025)
Biodiversity Net Gain Metric

List of objections 24/00938/FULLR3

- Angela Bridge
- Charlie Dixon
- Dr Alessio Checconi
- Dr Flora Dunster
- Dr Flora Spiegel
- Dr Lucy Peel
- Ed Bucknall
- Heidi Daish
- Jenny Ridgwell
- Miss Agnieszka Targonska
- Miss Claire Russell
- Miss J Pearce
- Miss Josephine Cook
- Miss Lesley McDonnell
- Miss Lucy Sladdin
- Miss Marie-Louise Plum
- Mr Angus Stewart
- Mr Duncan John
- Mr G Phillips
- Mr Gabriel Buttimore
- Mr Garry Marsland
- Mr Howard Sinden
- Mr Jack Engeham
- Mr Jack James
- Mr Jaime Rory Lucy
- Mr John Sinden
- Mr Kevin Cox
- Mr Liam Sharpe
- Mr Mark Sowden
- Mr Richard Hemery
- Mr Richard Potts
- Mr Rob Wonnacott
- Mr Scott Wescombe
- Mr Sean Clarke
- Mr Stephen Engeham
- Mr Stuart Picknell
- Mr Tim Miller (Society of Thames Mudlarks & Antiquarians)
- Mrs Alison Picknell
- Mrs Billie Bond
- Mrs Elzbieta Anderson

- Mrs Esther Cook
- Mrs Gail Howell
- Mrs Helen Marsland
- Mrs Kerry Hoppett
- Mrs Maarina Budd
- Ms Amy Holroyd
- Ms Amy Sinden
- Ms Caroline Nunneley
- Ms Ellen O'Keeffe
- Ms Gemma Duncan
- Ms Julie Johns
- Ms Monika Buttling-Smith
- Ms Rachel Walters
- Ms Shelley Murray
- Ms Susan Thompson
- Ms Susan Tomlin
- River Residents Group (Michelle Lovric)
- Sam Caethoven
- Sherry Babbitt
- Suzanne Toman
- Tony Smith

List of Statutory or Other Consultee Responses

- Air Quality Officer
- Environment Agency
- Environmental Health Officer
- Historic England
- Historic England GLAAS
- Lead Local Flood Authority
- London Borough of Southwark
- Network Rail
- Port of London Authority

APPENDIX B: Relevant Policies of the Development Plan

Relevant London Plan Policies

Policy GG1 Building strong and inclusive communities
Policy GG2 Making the best use of land
Policy SD4 The Central Activities Zone (CAZ)
Policy D3 Optimising site capacity through the design-led approach
Policy D4 Delivering Good Design
Policy HC1 Heritage conservation and growth
Policy HC3 Strategic and Local Views
Policy HC4 London View Management Framework
Policy G5 Urban greening
Policy G6 Biodiversity and access to nature
Policy SI 1 Improving air quality
Policy SI 13 Sustainable drainage
Policy T4 Assessing and mitigating transport impacts

Relevant Local Plan Policies

CS9 Thames and the Riverside
CS10 Design
DM 10.1 New development
DM 10.4 Environmental enhancement
CS12 Historic Environment
DM 12.1 Managing change affecting all heritage assets and spaces
DM 12.4 Ancient monuments and archaeology
CS15 Sustainable Development and Climate Change
DM 15.1 Sustainability requirements
DM 15.5 Climate change resilience and adaptation
DM 15.6 Air quality
DM 15.7 Noise and light pollution
CS16 Public Transport, Streets and Walkways

DM 16.1 Transport impacts of development
DM 16.2 Pedestrian movement
DM 16.8 River transport
DM 17.2 Designing out construction waste
CS18 Flood Risk
DM 18.1 Development in the City flood risk area
DM 18.2 Sustainable drainage systems (SuDS)
DM 18.3 Flood protection and climate change resilience
DM 19.2 Biodiversity and urban greening

Relevant City Corporation Guidance and Supplementary Planning Documents (SPDs)

The Thames Strategy SPD (2015);
Protected Views SPD (January 2012)
City of London Biodiversity Action Plan (2021)

Relevant Draft City Plan 2040 Policies

Strategic Policy S1: Healthy and Inclusive City
Policy HL1: Inclusive buildings and spaces
Policy HL2: Air quality
Policy HL3: Noise
Strategic Policy S8: Design
Policy DE1: Sustainable Design
Policy DE2: Design Quality
Policy VT1: The impacts of development on transport
Policy VT3: Vehicle Parking
Policy VT4: River Transport
Strategic Policy S13: Protected Views
Policy OS2: Urban Greening
Policy OS3: Biodiversity

Policy OS4: Biodiversity Net Gain

Strategic Policy S15: Climate Resilience and Flood Risk

Strategic Policy S17: Thames Policy Area

SCHEDULE: CONDITIONS	
1.	<p>Time Limit</p> <p>The development hereby permitted shall be begun before the expiration of three years from the date of this permission.</p> <p>REASON: To ensure compliance with the terms of Section 91 of the Town and Country Planning Act 1990</p>
2.	<p>Approved Plans</p> <p>The development shall not be carried out other than in accordance with the following approved drawings and particulars or as approved under conditions of this planning permission: Dwg nos Location Plan, 2401-BRL-02-XX-DR-C-0001 Rev T01, 2401-BRL-02-XX-DR-C-0002 Rev T01, 2401-BRL-02-XX-DR-C-0004 Rev T01, 2401-BRL-02-XX-DR-C-0007 Rev T01, 2401-BRL-02-XX-DR-C-0009 Rev T01 and 2401-BRL-01-XX-SK-C-0001 Rev P01.</p> <p>REASON: To ensure that the development of this site is in compliance with details and particulars which have been approved by the Local Planning Authority.</p>
3.	<p>Archaeological Written Scheme of Investigation</p> <p>No demolition or development shall take place until a written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. For land that is included within the WSI, no demolition or development shall take place other than in accordance with the agreed WSI, which shall include the statement of significance and research objectives, and</p> <p>A. The programme and methodology of site investigation and recording and the nomination of a competent person(s) or organisation to undertake the agreed works</p> <p>B. The programme and methodology for an archaeological foreshore survey</p> <p>C. The programme for post-investigation assessment and subsequent analysis, publication & dissemination and deposition of resulting material. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.</p> <p>REASON: To ensure an opportunity is provided for the archaeology of the site to be investigated and recorded in accordance with the following policy of the Local Plan: DM12.4</p>

4.	<p>Scheme For Protecting Nearby Residents</p> <p>Works shall not begin until a scheme for protecting nearby residents and commercial occupiers from noise, dust and other environmental effects has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be based on the Department of Markets and Consumer Protection's Code of Practice for Deconstruction and Construction Sites and arrangements for liaison and monitoring (including any agreed monitoring contribution) set out therein. A staged scheme of protective works may be submitted in respect of individual stages of the development process but no works in any individual stage shall be commenced until the related scheme of protective works has been submitted to and approved in writing by the Local Planning Authority. The development shall not be carried out other than in accordance with the approved scheme (including payment of any agreed monitoring contribution).</p> <p>REASON: To protect the amenities of nearby residents and commercial occupiers in accordance with the following policies of the Local Plan: DM15.6, DM15.7, DM21.3. These details are required prior to any work commencing in order that the impact on amenities is minimised from the time that development starts.</p>
5.	<p>Construction Environmental Management Plan (CEMP)</p> <p>Prior to the commencement of the development, a Construction Environmental Management Plan (CEMP) shall be submitted to and approved by the local planning authority. The CEMP shall include (but not limited to) details of:</p> <ul style="list-style-type: none"> - The method of construction; - Biosecurity measures to prevent the introduction of contaminants or spread of any Invasive non-native species (INNS) within any required materials; and - Measures protecting locally and nationally protected species of conservational concern during construction. <p>The demolition and construction works shall be carried out in accordance with the approved plan.</p> <p>REASON: To ensure that construction works do not have an adverse impact on public safety, the transport network and the environment in accordance with London Plan Policies T7 and G6 and the following policies of the Local Plan: DM15.6, DM16.1 and DM 19.2. These details are required prior to construction work commencing in order that any impact is minimised from the time that construction starts.</p>
6.	<p>Non-Road Mobile Machinery Registration</p> <p>Prior to the commencement of the development, the developer/ construction contractor shall sign up to the Non-Road Mobile Machinery</p>

	<p>Register. The development shall be carried out in accordance with the Mayor of London Control of Dust and Emissions during Construction and Demolition SPG July 2014 (Or any subsequent iterations) to ensure appropriate plant is used and that the emissions standards detailed in the SPG are met. An inventory of all NRMM used on site shall be maintained and provided to the Local Planning Authority upon request to demonstrate compliance with the regulations.</p> <p>REASON: To reduce the emissions of construction and demolition in accordance with the Mayor of London Control of Dust and Emissions during Construction and Demolition SPG July 2014 (or any updates thereof), Local Plan Policy DM15.6 and London Plan Policy SI1D. Compliance is required to be prior to commencement due to the potential impact at the beginning of the construction.</p>
7.	<p>Biodiversity Net Gain Plan</p> <p>Notwithstanding the details shown on the approved plans and set out in the supporting information, details of all Biodiversity Net Gain shall be submitted to and approved in writing by the local planning authority prior to above ground works. Details shall include:</p> <ul style="list-style-type: none"> a) Completed Biodiversity Net Gain Plan b) Completed statutory metric with the Pre-development and post-development habitat values. <p>The development shall then be carried out in accordance with these approved details.</p> <p>REASON: In the interest of ensuring there is no net loss of habitats, no harm to species populations and to ensure the provision of biodiversity gain in accordance with the requirements of Schedule 7A of the Town and Country Planning Act 1990, the NPPF, London Plan policy G6 and Local Plan policy CS15 and DM 19.2.</p>
8.	<p>Landscape and Ecological Management Plan (LEMP)</p> <p>The development hereby permitted shall not commence until a detailed Landscape and Ecological Management Plan (LEMP) has been submitted to and approved in writing by the Local Planning Authority. The plan should include:</p> <ul style="list-style-type: none"> (a) details of proposed habitat enhancement/creation measures; (b) management details of any proposed habitat enhancement; and (c) details of how these habitat enhancement/creation measures would be secured for at least 30 years. <p>Such approved measures shall thereafter be implemented in full.</p> <p>Reason: To ensure a net biodiversity gain in accordance with Schedule 7A of the Town and Country Planning Act 1990, the NPPF, London Plan policy G6 and Local Plan policy CS15 and DM 19.2.</p>

Informatives:

1.	<p>NPPF</p> <p>In dealing with this application the City has implemented the requirements of the National Planning Policy Framework to work with the applicant in a positive and proactive manner based on seeking solutions to problems arising in dealing with planning applications in the following ways:</p> <ul style="list-style-type: none"> • detailed advice in the form of statutory policies in the Local Plan, Supplementary Planning documents, and other written guidance has been made available; • a full pre application advice service has been offered; • where appropriate the City has been available to provide guidance on how outstanding planning concerns may be addressed.
2.	<p>Good Archaeological Practice</p> <p>The written scheme of investigation will need to be prepared and implemented by a suitably professionally accredited archaeological practice in accordance with Historic England's Guidelines for Archaeological Projects in Greater London. This condition is exempt from deemed discharge under schedule 6 of The Town and Country Planning (Development Management Procedure) (England) Order 2015.</p>
3.	<p>Environment Agency Regulations</p> <p>The Environmental Permitting (England and Wales) Regulations 2016 require a permit to be obtained for any activities which will take place:</p> <ul style="list-style-type: none"> o on or within 8 metres of a main river (16 metres if tidal) o on or within 8 metres of a flood defence structure or culvert including any buried elements (16 metres if tidal) o on or within 16 metres of a sea defence o involving quarrying or excavation within 16 metres of any main river, flood defence (including a remote defence) or culvert o in a floodplain more than 8 metres from the river bank, culvert or flood defence structure (16 metres if it's a tidal main river) and you don't already have planning permission. <p>For further guidance please visit https://www.gov.uk/guidance/flood-risk-activitiesenvironmental-permits or contact our National Customer Contact Centre on 03702 2 422 549 or by emailing enquiries@environment-agency.gov.uk.</p> <p>The applicant should not assume that a permit will automatically be forthcoming once planning permission has been granted, and we advise them to consult with us at the earliest opportunity.</p>
4.	<p>River Works Licence (RWL)</p> <p>The applicant is reminded that prior to any works taking place a River Works Licence (RWL) will be required with the Port of London Authority.</p>

5.	<p>Network Rail</p> <p>The developer must ensure that their proposal, both during construction and after completion does not:</p> <ul style="list-style-type: none">o Encroach onto Network Rail lando Affect the safety, operation or integrity of the company's railway and its infrastructureo Undermine its support zoneo Damage the company's infrastructureo Place additional load on cuttingso Adversely affect any railway land or structureo Over-sail or encroach upon the air-space of any Network Rail lando Cause to obstruct or interfere with any works proposed works or Network Rail development both now and in the future.